

Proposed Artificial Grass Pitches at:
Heron Eccles Playing Fields,
Abbotshey Avenue,
Liverpool,
L18 7JS.

Transport Statement

Date 20th July 2016

Kier Business Services

1st Floor Station House, Exchange Station, Tithebarn Street, Liverpool, L2 2QP

T: 0151 600 5500 | www.kier.co.uk

Kier Business Services Limited | Registered in England No. 3679828

Registered Office: Tempsford Hall, Sandy, Bedfordshire, SG19 2BD

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1 Introduction

1.0 Overview / Background

1.1.1 This Transport Statement has been produced by Mouchel on behalf of Liverpool City Council (LCC) to provide highways and transportation advice in support of a planning application for the Parklife Development works at the Heron Eccles Playing Fields site and new vehicular and pedestrian access arrangements and car parking facilities to be provided as part of the scheme.

1.1.2 The Heron Eccles Playing Fields site is being developed as part of joint FA and Liverpool City Council 'Parklife' initiative to provide a number of new 3G Artificial Grass Pitches and associated Changing Pavilion facilities at four sites across the city. The proposed development works at this site include the provision of three full sized fenced and floodlit 3G artificial grass pitches (AGPs), extension of the existing pavilion building to provide new entrance and reception facilities, club room and space for coach education together with associated site access improvements and enlarged car parking area. The approximate site area is 14.3Ha (35.3 Acres) while the proposed extension will provide 255.6 sq.m of additional floor space.

1.1.3 This development supports both the local authority's city-wide strategy to improve access to physical activity and sport and the FA's national strategies aimed at improving access to year round, high quality footballing facilities. The pavilion, AGPs and grass pitch provisions are designed to increase participation and raise the level of footballing skill in players, coaches and referees. For this reason the extended pavilion building also includes space for club, social and educational use.

1.1.4 The existing car park is large, formed mainly in tarmac with a later 'overspill' area of plastic reinforced gravel, and currently accommodates a maximum of about 164 cars subject to how the car park is used. There was evidence on busy match days that a number of cars were parked outside delineated car parking bays, on grassed verges and part on/part off pavements adjacent to main car parking areas – See Photos 11 and 12. Such inconsiderate parking can cause issues with regards to smooth traffic movement into and out of the site and could potentially restrict emergency vehicle access to the pitch areas. During periods of high usage of pitches (mainly Saturday and Sunday match days) the main roadways (Abbotshey Avenue and adjacent streets) fronting or near to the site can be lined either side of the road by significant numbers of vehicles parked 'half-on/half-off' pavements which constitutes a potential hazard to road users and pedestrians alike (Photos 7, 8 & 13).

1.1.5 As part of the proposed development works the existing car park area is to be significantly enlarged to provide capacity for 283 vehicles to park within the site boundary, with a view to mitigating parking congestion on the roads outside the Heron Eccles Playing Fields site on match days and during the week when AGPs will be utilised throughout the day, thus providing safer access to the facilities on site. The proposed car park layout, access off Abbotshey Avenue and highway impact will be reviewed in this brief Transport Statement (TS).

1.1.6 The scope of this TS has been agreed with the local highway authority through email correspondence, which for information purposes is included within Appendix A.

1.1.7 This proposal at the Heron Eccles Playing Fields site is one of several City Football Hub projects being developed in Liverpool. It is proposed that the new footballing facilities will still to be accessed from Abbotshey Avenue but via improved access and pavement crossing in the current location, incorporating replacement vehicular and pedestrian gates within the proposed new boundary fencing to ensure separation of pedestrians and vehicles when accessing the site.

2 Existing Site Context

2.1 Site Location and Configuration

2.1.1 The Heron Eccles Playing Fields site is located approximately 6000m south east of Liverpool City Centre and approximately 2000m north of Garston.

2.1.3 The Heron Eccles Playing Fields site is bounded on all sides by residential areas with a railway line bounding the site along the south west boundary. The adjacent Abbotshey Avenue, Greenleigh Road and Greenlake Road streets link this residential area to Mather Avenue and Aigburth Road which are main arterial routes into and out of Liverpool.

See Figure 1: Site Location

2.1.4 Figure 2 shows the location of the existing car park access off Abbotshey Avenue, to north elevation of the site. The existing car park is accessed via pavement crossing and gates shown in Photos 3, 4 and 5.

2.1.5 The existing car park encompasses the following arrangement of spaces:

- Facility for approx. 164 maximum car parking spaces (marked)

2.1.6 Currently the existing car park often operates over capacity on match days with cars occasionally blocking one another in order to park within the sports ground boundary (Photo 11). This could potentially cause issues in terms of emergency vehicle access. Cars were also seen to be parking on grassed areas adjacent to the main car park as seen in Photo 12 as 'informal overspill' on areas not suitable for this purpose.

2.1.7 On weekend match days the pavement side of both carriageways either side of Abbotshey Avenue and Greenleigh Road can be lined with significant numbers of vehicles parked 'half-on/half-off' pavements which constitutes a potential hazard to road users and pedestrians alike – See Photos 7, 9 and 13.

2.1.8 Discussions with Liverpool City Council and MYA staff (who currently operate the site) revealed that the informal nature of parking on match days could potentially cause blocking in of cars and hindrance to emergency vehicles trying to access pitch areas. This may cause serious issues during an emergency incident.

2.2 Transport Network

2.2.1 The Heron Eccles Playing Fields site is well connected in terms of the surrounding pedestrian infrastructure. Footways are adequate and reasonably well maintained on Abbotshey Avenue, Greenleigh Road or Greenlake Road, although instances of thoughtless parking on grass verges and pavements can cause issues to pedestrians particularly when attempting to cross Abbotshey Avenue and Greenleigh Road from far side of the roadway to the site. Pedestrians' vision can be restricted when crossing the road by cars parking part on the pavement, part on roadway down both sides of these carriageways.

Abbotshey Avenue and adjacent roadways are subject to a speed limit of 20mph due to residential nature of area.

2.2.2 We are aware of the current Liverpool Cycle Route Network and the continuing development of an extensive network of cycle routes as part of the City's Cycling Strategy (Liverpool's Cycling Revolution – a Cycling Strategy for Liverpool 2014-26). The proposed site sits not far from several of these designated cycle routes and as a result it is anticipated that the number of people accessing the site by cycle may increase over time.

2.2.4 Although it is highly likely that many of the prospective users will arrive by car or cycle many may choose to use public transport. The site is reasonably well placed for bus stops on both Mather Avenue and Rose Lane and these are serviced by bus routes 86, 86A and 86 (Mather Avenue) and 61, 80, 80A, 173 and 201 (Rose Lane).

2.2.5 Mossley Hill and West Allerton railway stations are within reasonable walking distance of the Heron Eccles Playing Fields site (approx.1000m walk) as can be seen in Figure 1. Four services per hour can be made to Liverpool Lime Street Station from both stations.

2.2.6 As can be seen in this section the Heron Eccles Playing Fields site is well served by the surrounding local transport network for walking, cycling, bus travel and rail travel.

3 Proposed Development

3.1 Overview

3.1.1 The proposed enlarged and fully resurfaced car park which will accommodate 283 vehicle spaces can be seen in Figure 14. The car park is to be located as shown with improved vehicular and pedestrian access provided off Abbotshey Avenue, as can also be seen in Figure 14.

3.1.2 The proposed car park encompasses the following arrangement of spaces:

- 13 Accessible Spaces
- 270 Standard Spaces
- 5 Cycle Spaces

3.2 Parking Policy

3.2.1 With regards to planning policy the Liverpool City Council 'Ensuring a Choice of Travel SPD' outlines 1 parking space per two site users based on number of players, team managers and officials utilising the facility at peak times or to full capacity.

3.2.2 The standards described in the document are set out to ensure that car parking requirements in general are kept at a level so as to encourage sustainable transport usage and to ensure safe access to facility users by all means of transport. As such the Heron Eccles Playing Fields site car park proposal is in line with the policy providing sufficient spaces to ensure adequate parking is provided on site together with easy pedestrian and cycle access for those arriving by bike, on foot and by public transport. In this context, this will significantly reduce if not completely negate any parking on Abbotshey Avenue and adjacent streets during peak times of use such as weekend match days.

3.2.3 With regards to accessible parking the SPD outlines that a minimum of 6% of the first 100 parking spaces in a development should be reserved for blue badge holders. Thereafter, the number of spaces are negotiable. The proposed car park will have 283 vehicular spaces with 13 accessible car parking spaces allocated within the enlarged car park which meets SPD requirements.

3.3 Trip Generation

3.3.1 As the proposed development will spread the number of cars visiting and leaving the site over an extended time frame the density of site traffic in the area should be reduced and with significant additional on-site parking being provided current issues with on-site parking during match days should be significantly (if not completely) mitigated. It is anticipated that the reduced overall traffic flow and density on the residential Abbotshey Avenue and adjacent roadways will reduce congestion and parking issues along residential streets in the area. The significantly enlarged and improved car parking facilities will provide adequate spaces for the Heron Eccles Playing Fields facility users through the day and evenings preventing blocking issues previously seen in the existing car park, negating the occurrence of parking along adjacent roads and as stated before, making the building/facilities more easily and safely accessed from Abbotshey Avenue via improved bellmouth and gates.

3.3.2 It is envisaged that the car park will generate at approximately 70% capacity on busy match days thus generating up to 200 trips during peak usage, e.g. on weekend match days (in the event that all facility users and visitors leave during these times).

3.3.3 200 trips across an hour equates to less than seven vehicles every 2 minutes. It is not envisaged that this number of vehicles will have a significant impact on the road network. The impact would be considered negligible (if not reduced) considering current numbers of facility users and car parking on match days at present.

3.3.4 The Applicant seeks permission to use and operate the AGPs (including floodlighting as needed) between the hours of 10.00 to 21.00 Monday to Fridays, from 09.00 to 18.00 on Saturdays, Sundays and Bank Holidays.

3.3.5 The pavilion is to remain open for a further half hour until 21:30 Monday to Friday (and until 18.30 at weekends and Bank Holidays) to allow players to shower, change and exit the site safely. These hours of operation are consistent with other community accessible facilities in Liverpool.

3.4 Impact on Roads adjacent to Site Entrance

3.4.1 Whilst it is anticipated the additional parking provided will address the immediate impact on the surrounding residents, the actual impact will not be fully identified until the development is operational and so an access strategy will form part of the future operation of the site.

3.4.2 Discussions with LCC Highways has concluded that an Access Strategy will be recommended as a condition relative to any future planning approval for the site. The purpose of this strategy (including signing) will be to dilute the traffic impact from vehicles accessing the site from north and south. This will be an evolving process as some options may not be appropriate. In this context, the efficacy of this Access Strategy will be reviewed periodically. Furthermore LCC Highways has undertaken a consultation exercise with local residents to consider a number of options to deter on-street parking on surrounding roads to the site including; residents parking, one-way systems and 'H' brackets etc. Whilst it is evident from this consultation exercise that residents parking permits was the preferred solution it is recommended that the introduction of mitigating traffic management measures be considered on a phased approach. Dialogue is currently ongoing with local Ward Councillors and residents groups to determine how this should be implemented with certain measures, such as H-markings to protect driveways potentially being introduced prior to the development becoming operational, and other measures being considered once the site is fully operational.

3.4.3 In addition to the LCC Highways consultation, there have also been two public meetings with local residents to discuss the wider implications to the proposed development of the site. Notwithstanding any recommended highway planning conditions (Access Strategy) relative to the development of the site, there will be a requirement of the future operator of the site to provide a degree of car parking supervision during peak-periods of operation. It will also be a requirement of the incoming operator to continually liaise with local residents to identify and resolve any developing traffic implications relative to the use of the site by patrons.

3.4.4 It is not envisaged that large concentrations of coaches and buses will visit the site on a regular basis. This is due to the restricted nature of site access and unsuitability of roads adjacent to site entrance for coach access. Therefore on the occasions when coaches or buses arrive it is proposed to define a suitable drop-off point on the wider Greenhill Road (close to where Greenleigh Road meets Greenhill Road) where coaches can safely drop-off and pick-up players, teams, etc. using the pitch facilities at Heron Eccles Playing Fields without causing issues on roadways adjacent to site entrance. The Access Strategy shall identify a suitable layover point

for coaches after dropping off and would involve a location that would not cause any issues to residential amenity.

3.5 Access

3.5.1 Access for the new extended car park will be located at the current vehicular gate position but the pavement crossing and associated bellmouth will be upgraded in line with Highways guidance. The new upgraded vehicular and pedestrian gates will remain in locations similar to existing double palisade gates with separate vehicular and pedestrian access provided to ensure safe access to the site and a defined pedestrian access route incorporated into the design.

3.5.2 The current visibility splay will not be affected by the proposed pavement crossing improvements at the site entrance and proposed gates will be set back to existing line providing a 5.8m long apron for cars 'queueing' to access the site. By provision of an extended large car parking area within the site boundary there should be no requirement for facility users or visitors to park on either Abbotshey Avenue, Greenleigh Road or Greenlake Road thus mitigating any reduction of visibility which can currently occur on weekend match days with cars often seen parking on pavement and verges adjacent to pavement crossing access.

3.6 Vehicle Tracking

3.6.1 Vehicle tracking of the proposed car park has been undertaken using a Large Car (1.872m Width, 5.079m Length) in Auto Track.

4 Road and Pedestrian Safety

4.1 General

4.1.1 The proposed development will not increase traffic flows in the area as overall pitch numbers are being reduced so there should be fewer cars using the facilities at any one time, even on match days, as use of fewer pitches (both artificial and grass) will be spread out over a longer period of time.

4.1.2 The provision of a large extended car parking area within the site boundary, and site access via improved bellmouth and separate pedestrian and vehicular gates will ensure safe access to facilities for pedestrians and cars alike, and should negate any requirement for parking along Abbotshey Avenue and adjacent streets (eg. Greenleigh Road and Greenlake Road) thereby improving road safety in the areas adjacent to the site.

5 Summary

5.1.1 Mouchel was appointed by Liverpool City Council (LCC) to provide highways and transportation advice in support of a planning application for the Parklife Development works at the Heron Eccles Playing Fields site and new vehicular and pedestrian access arrangements and car parking facilities to be provided as part of the scheme.

5.1.2 The existing tarmac and gravel car parking areas although significant in area can be short of capacity on weekend match days (current provision for approx. 164 cars) leading to occasional problems with parking along Abbotshey Avenue and adjacent streets, restricting carriageway width and causing a potential hazard to pedestrians and cars alike. The existing car park will be extended to provide approx. 119 more car parking spaces and the area will be resurfaced in tarmac with marked parking bays to facilitate efficient use of parking areas thus reducing parking issues in streets adjacent to the site on match days. The number of spaces to be provided reflects anticipated usage patterns and demand – see Appendix B.

5.1.3 Currently the existing car park operates over capacity at peak times with cars often blocking one another in order to park within the playing field grounds. Discussions with MYA revealed that the blocking in of the car park causes issues during match days and could restrict emergency vehicle access if an ambulance was required to go on the pitch areas.

5.1.4 As a result of the proposed car park extension and improvement works an additional 11 accessible spaces and 108 standard spaces (providing 283 spaces in total) will be made available.

5.1.5 The proposed number of parking spaces for both standard and accessible spaces is in line with current Liverpool planning standards.

5.1.6 It is envisaged that the car park will generate at approximately 70% capacity on busy match days thus generating up to 200 trips during peak usage, e.g. on weekend match days (in the event that all facility users and visitors leave during these times). 200 trips across an hour equates to less than seven vehicles every 2 minutes. It is not envisaged that this number of vehicles will have a significant impact on the road network, the overall impact would be considered negligible.

5.1.7 The visibility splay of the proposed car park access off Abbotshey Avenue is not affected by the proposed works and current occasional issues with parking on the main road adjacent to the site entrance adversely restricting sightlines should largely be resolved, making the site safer to access and egress by car, cycle and on foot. Double yellow lines near the site entrance and H brackets to front of residents' properties are an option to keep sightlines clear when accessing/egressing the site and to avoid potential 'blocking in' or restriction of residents' driveways.

5.1.8 Vehicle tracking undertaken for the proposed car park showed that entry, parking and exit could be undertaken safely and without infringing on any of the other car parking spaces in the car park.

5.1.9 The development is not increasing traffic flows in the area as site user numbers at any particular time will not increase (pitch numbers being reduced); as a result it is not considered that the development will have any material negative impact on road safety in the area. In fact there should be a significant improvement in road safety with the use of on-site football facilities spread over a longer timescale, the large extended car park providing significantly more parking spaces within the site boundary thus reducing (if not totally negating the need for) on-street parking on the adjacent

roadways and pavements, and providing improved vehicular and separate pedestrian access to the Heron Eccles Playing Fields site and facilities. With the new development there will be a reduction in overall pitch numbers and therefore a reduction in the maximum no. of users visiting the sites at peak times. By providing improved and extended parking facilities at this site the proposed development works can only be considered beneficial in terms of overall parking and traffic management generally and especially at peak periods with current match day parking issues potentially resolved or mitigated to a large degree.

5.1.10 Furthermore the site is well served by the surrounding local transport network for walking, cycling, bus travel and rail travel.

5.1.11 An Access Strategy will be developed to mitigate and provide solutions to potential traffic impact on the development site. This Access Strategy will be reviewed periodically.

5.1.12 As such it is the view of this report that there are no overriding reasons preventing the highway authority from recognising that the proposals are acceptable in transport terms.

APPENDIX A

John Jager

From: John Jager
Sent: 02 December 2015 11:11
To: 'Taylor, Mike'
Cc: 'Dingwall, Andy'; 'Kearney, Michael'; 'Wiggins, Ian'
Subject: RE: 2015 11 30 Parklife Projects at 6 No. Playing Field sites in Liverpool

Mike,

It was good meeting Monday and discussions were very useful.

Summarising discussions (omitting Wavertree and Garston comments as these sites are no longer being considered as part of project) –

1. Car parking provision requirements to be assessed on basis of 1 car parking space per two users ('users' being no. of players, team managers and officials utilising the facility at peak times or to full capacity). Client to provide details of maximum user numbers before and after completion of the proposed development works at each site.
2. Accessible parking bays (in bitmac construction) to constitute a min. proportion of 6% of overall parking bay provision at the relevant sites and be located adjacent to pavilion facility as far as possible. Client to confirm that 6% parking (or greater?) is to be provided as accessible bays.
3. Mouchel/LCC to submit a 'Transport Statement' as part of Planning Application outlining existing facilities (no. of pitches and other facilities on or adjacent to site requiring access/car parking) and current car parking provision together with details of revised facilities subsequent to development works and intended car and cycle parking provision to be provided as part of the scheme proposals at the various sites. I have previously asked the LCC Client for any previous traffic or access surveys undertaken at the various sites to assess current usage (at peak times) and adequacy of car parking, cycle stands and access 'off road' as currently provided. Details will be submitted to support application if and when available.
4. Access to sites to be provided by means of two way access facility, suitably wide to facilitate flow of traffic in and out of site entrance (subject to available width of opening, eg. Heron Eccles entrance very narrow with limited scope to widen. Secondary pedestrian access gates should be provided at relevant sites (eg. Heron Eccles, Simpson and Jericho Lane where new and improved access and parking facilities are to be provided) again subject to available potential width of opening. You are to check that pavement crossings and new site access aprons are to be designed by Amey under instruction from LCC Client (Section 278 Agreement not required?). Do Amey survey relevant access and crossover locations to inform design and advise on whether any trees, etc, need removing to facilitate sightlines when accessing/egressing site (eg. trees at Simpson and Jericho Lane?)
5. Full 'bellmouth' type access into sites preferred but not essential. Simpson has greater than recommended 5m apron length between gates and road. Heron Eccles access is very narrow and restricted with little scope to widen (subject to survey).
6. With new pitch installations there is a consequent reduction in overall pitch numbers and therefore the maximum no. of users visiting the sites at peak times. By providing massively improved and extended parking facilities at each site the proposed development works can only be considered beneficial in terms of overall parking and traffic management generally and especially at peak periods with current matchday parking issues potentially mitigated to a large degree.
7. Principle agreed to provide 5 cycle 'toast rack' type parking facilities at each site as part of overall proposals. Is this in line with LCC/FA/FF requirements for such sites?

Any comments or additional issues not covered above please do not hesitate to call me.

Regards,
John.

John Jager

APPENDIX B

John Jager

From: Kearney, Michael <Michael.Kearney@liverpool.gov.uk>
Sent: 09 December 2015 14:24
To: John Jager
Cc: Wes Bugg; John Marshall; Chris Smith
Subject: RE: 2015 12 03 Client options - Comments - Changes to Brief

Hi John

Proposed layouts look ok. However we need to take cognisance of the required 2no. rugby pitches at Jericho Lane. The H&F layout at Jericho is my preferred location.

Also the pavilion at Heron Eccles will require a 'bolt-on' social/meeting room of a size similar to the room in the modular building layout.

In terms of car parking I have set out below a quick scenario which will determine numbers of spaces etc.

Scenario 1 – Adult football on 3 x AGP's

120 players and officials
100 spectators
==

220 people + 50% (110) overlap for next game = 330 people – say 165 car park spaces + 25% for H&F = 206 spaces

Scenario 2 – Junior football on 3 x AGP's

6 games 9 v 9 = 130 players and officials
200 spectators
==

330 + 50% overlap for next game = 495 people – say 250 car park spaces

The main issue will be when junior football is taking place at weekends over a 3 to 4 hour period with games kicking off on the hour or half-hour etc. which attracts a significant number of vehicles. In this instance the optimum car parking spaces required will be circa 200 at each site.

Therefore I have set out below my calculations for each site:

Jericho Lane 200 new spaces
Jeffrey Humble 200 spaces in total (mix of existing and new)
Heron Eccles 200 spaces in total (mix of existing and new)
Simpson Ground 200 new spaces

Regards

Mike

APPENDIX C

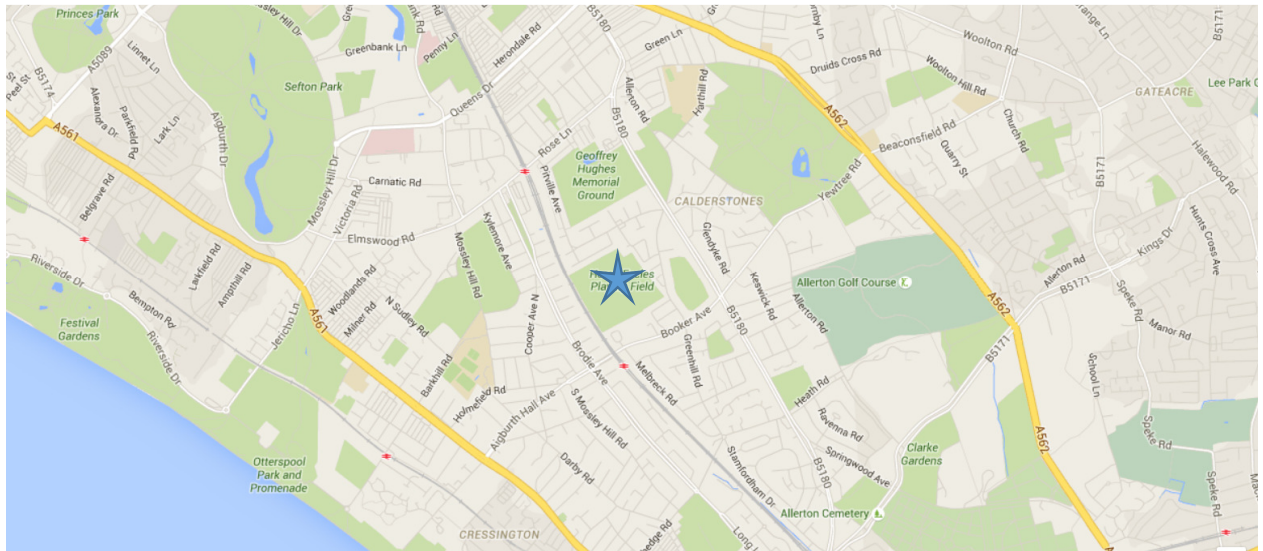


Figure 1: Site Location



Figure 2: Existing Site Layout



Photo 3 – Existing site access and car park beyond from Abbotshey Avenue



Photo 4 – Existing site access and car park beyond from existing pavilion car park side



Photo 5 - Existing tarmacadam car park areas in foreground, permeable overspill car park beyond



Photo 6 - Existing car park areas and site access gates taken from permeable parking area



Photo 7 - Parking along both sides of Abbotshey Avenue on typical match day



Photo 8 – Parking along both sides of Abbotshey Avenue on typical match day (car on pavement)



Photo 9 – Parking adjacent to site entry on typical match day (restricting driveway access in places)



Photo 10 – Parking adjacent to site entry on typical match day (some parking on grass verges)



Photo 11 – Full car park on matchdays with significant parking on verges outside designated bays



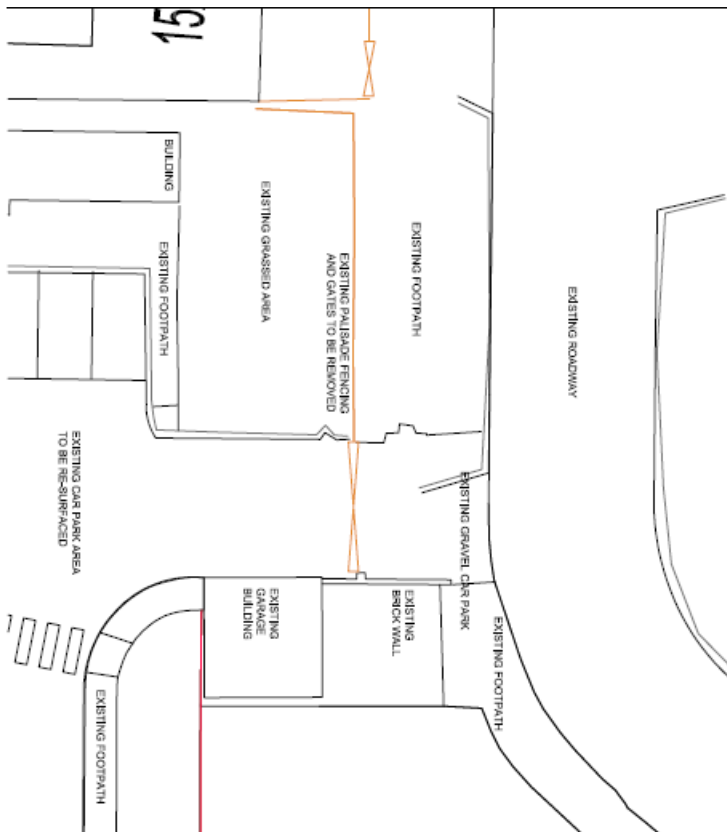
Photo 12 – Parking on grassed areas adjacent to car parking areas on typical matchday



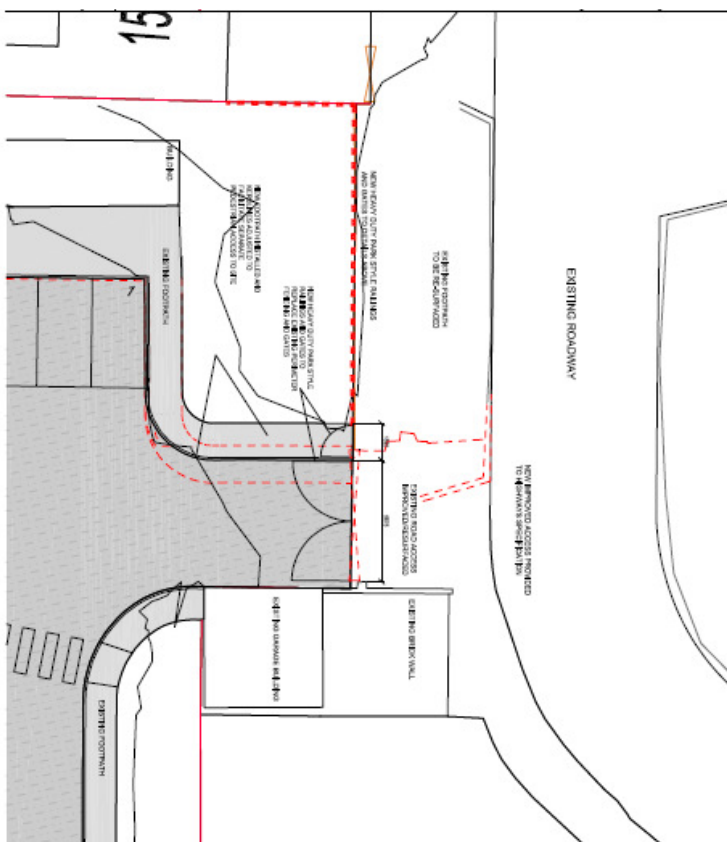
Photo 13 – Parking on Greenleigh Road (opp. Site entrance) on typical match day (restricting access and sight lines)



Figure 14 – Proposed Site Layout showing Extended Car Park Area



Existing Site Access Layout



Proposed Site Access Layout

Figure 15 – Proposed Upgrading of Access to Site
(See Dwg 1070311-MOU-00-DR-A-0004)