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INTRODUCTION

1.1 Introduction

This Landscape and Public Realm Statement has been prepared by Planit-IE and forms part of the the Full Planning Application submitted on behalf of Romal Capital.

The proposals are for a new residential regeneration scheme within Liverpool Waters.

The landscape proposals for C02 have been designed in accordance with a number of key drivers specific to the overall vision and masterplan for Liverpool Waters, such as the following:

- A portion of the Canal Way Promenade along West Waterloo Dock is included in the design of Plot C02, facilitating the strategic connection between the Princes Dock and Central Docks Neighbourhoods.
Although C02 is standalone, it doesn't preclude the delivery of the Canal Way Promenade linking the neighbourhoods - the landscape scheme is working hard to ensure the route is a success. Providing a wide and legible pedestrian and cycle route along West Waterloo Dock. This proposed new public route is strategically important to the Liverpool Waters Masterplan, opening up the site for the public benefit and forming a catalyst for future development.
- Materials will be site appropriate, robust and provide an aesthetic in keeping with the industrial heritage and are in accordance with the Central Docks Neighbourhood Masterplan. They reflect the adjacent materials proposed on the Isle of Man terminal and Link Road and will be site appropriate.

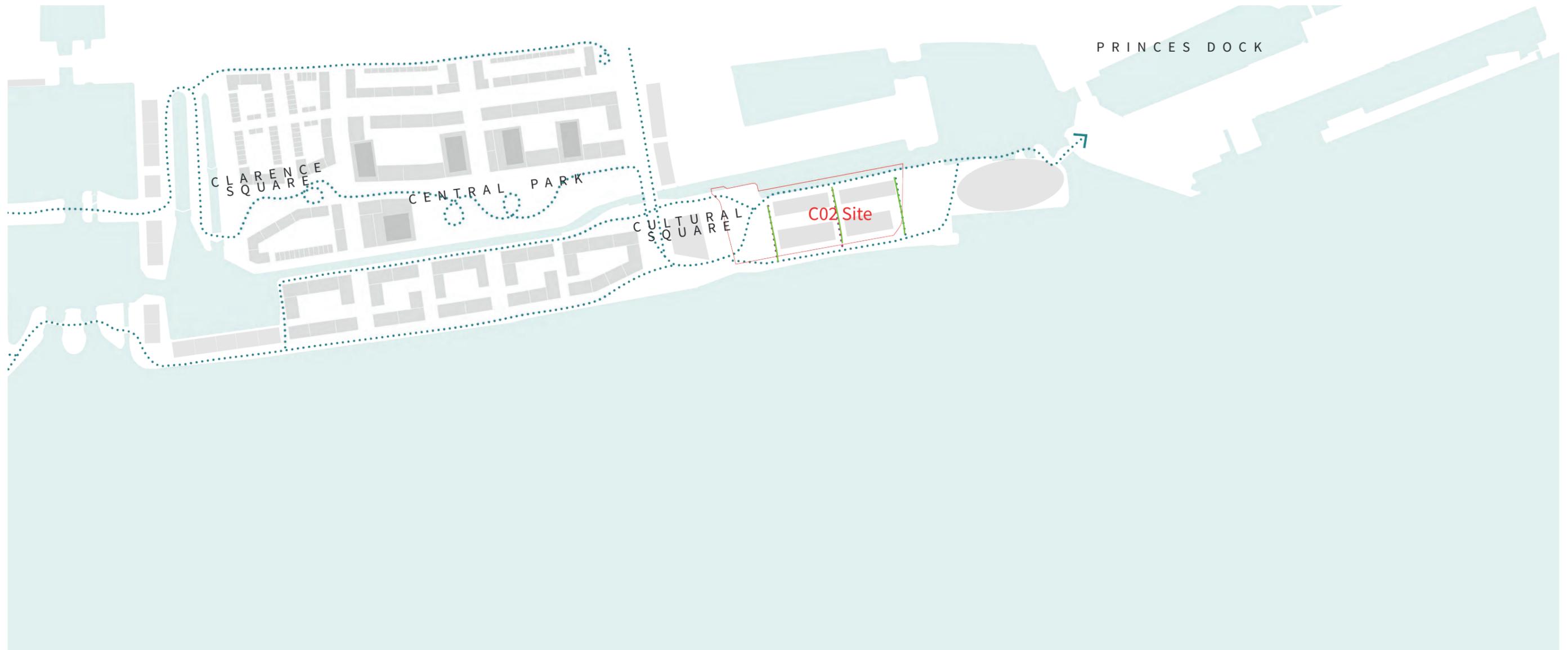
The landscape and urban realm proposals include:

- Two commercial units with external cafe seating along the water's edge.
- Linking spaces between the buildings for residential recreation and connectivity.
- Timber floating jetties for the potential of possible water taxis and small boats temporary berthing.
- A generous pedestrian and cycle route with seating to guide visitors from the proposed northern link road to the West Waterloo Dock waterside.

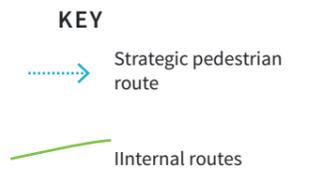


EXISTING PHOTOGRAPH FROM SITE LOOKING NORTH

1.2 Liverpool Waters indicative pedestrian movements

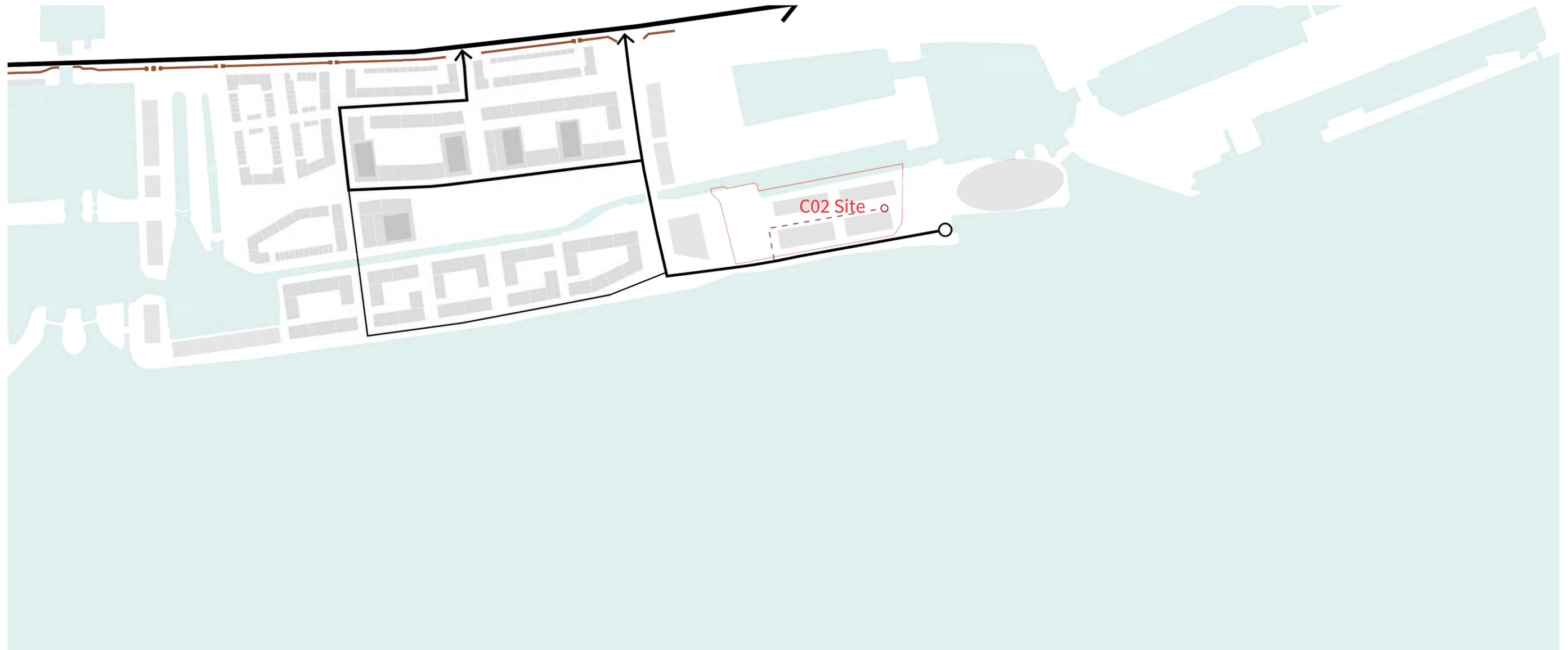


INDICATIVE PEDESTRIAN ROUTES ACROSS LIVERPOOL WATERS



The following diagrams provide an indicative set of strategies across the Liverpool Waters Masterplan area.

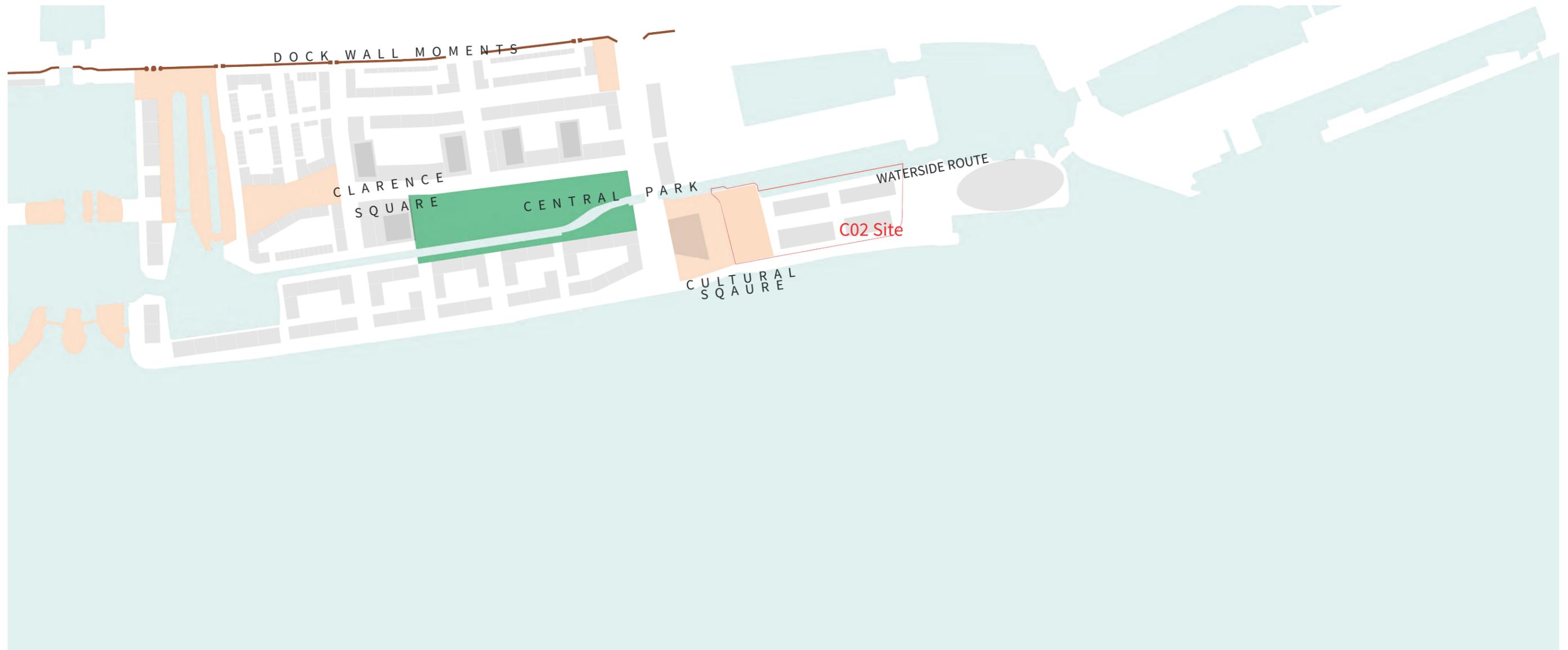
1.3 Liverpool Waters indicative vehicle movements



INDICATIVE VEHICLE ROUTES ACROSS LIVERPOOL WATERS

- KEY**
-  Dock road
 -  Primary Internal route
 -  Internal route

1.4 Liverpool Waters indicative public open space



INDICATIVE PUBLIC OPEN SPACE ACROSS LIVERPOOL WATERS

1.5 Liverpool Waters indicative interactions with the water



INDICATIVE WATER ACTIVITIES ACROSS LIVERPOOL WATERS

KEY

- Water activity route
- Recreational activity within water
- /// Interaction with water

1.6 Liverpool Waters indicative cycle network/public transport



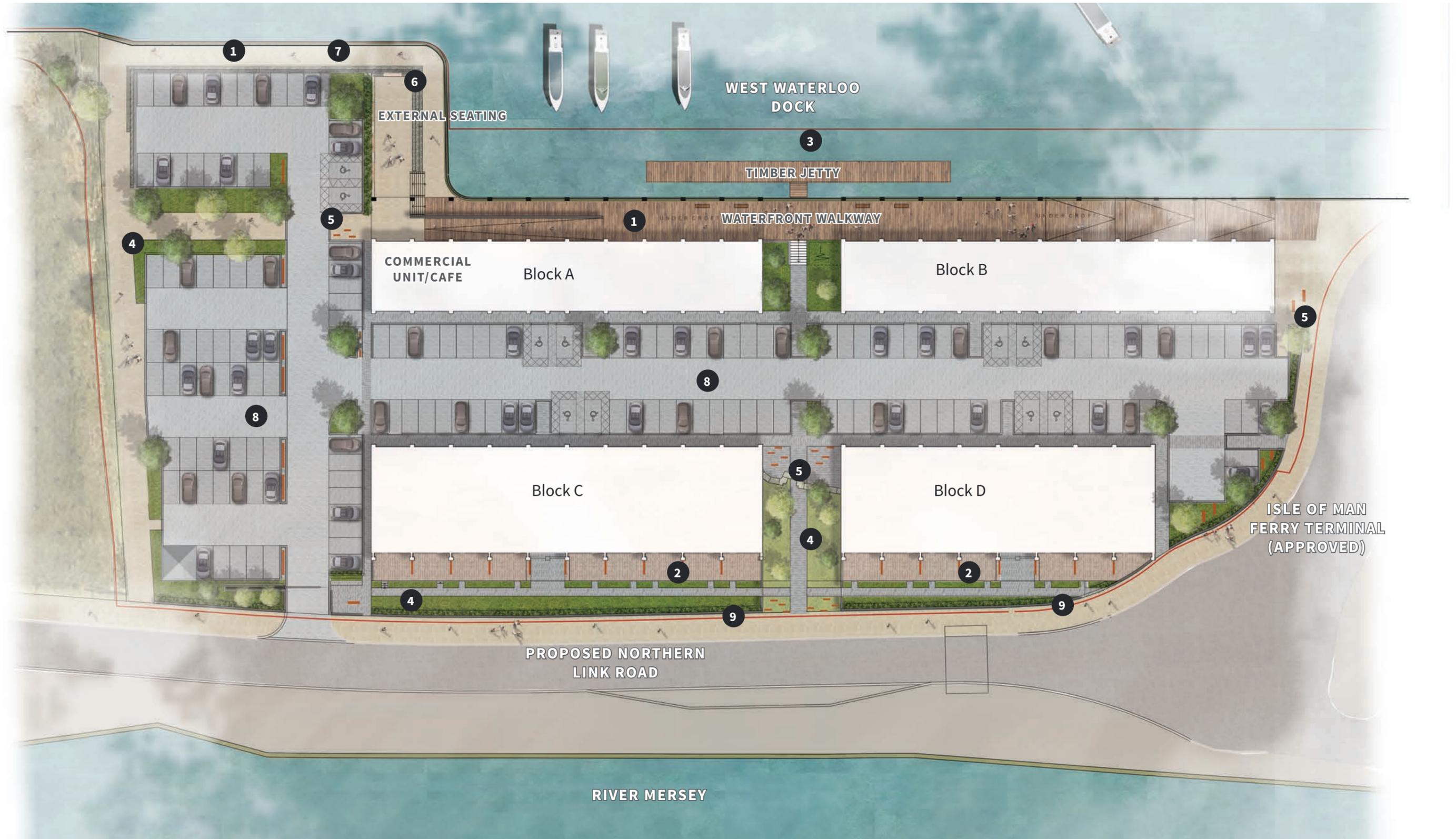
INDICATIVE CYCLE NETWORK/PUBLIC TRANSPORT ROUTES ACROSS LIVERPOOL WATERS

KEY

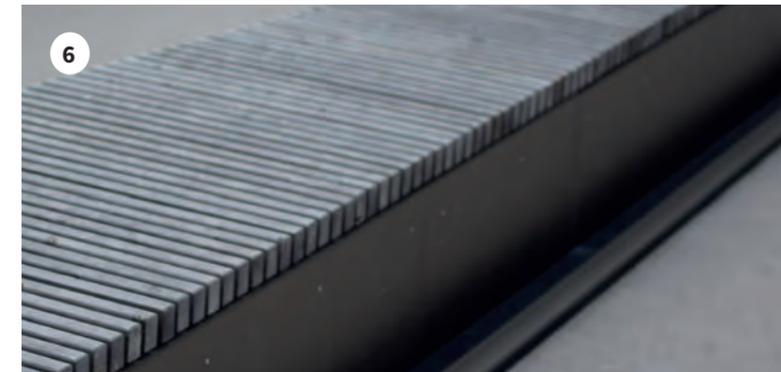
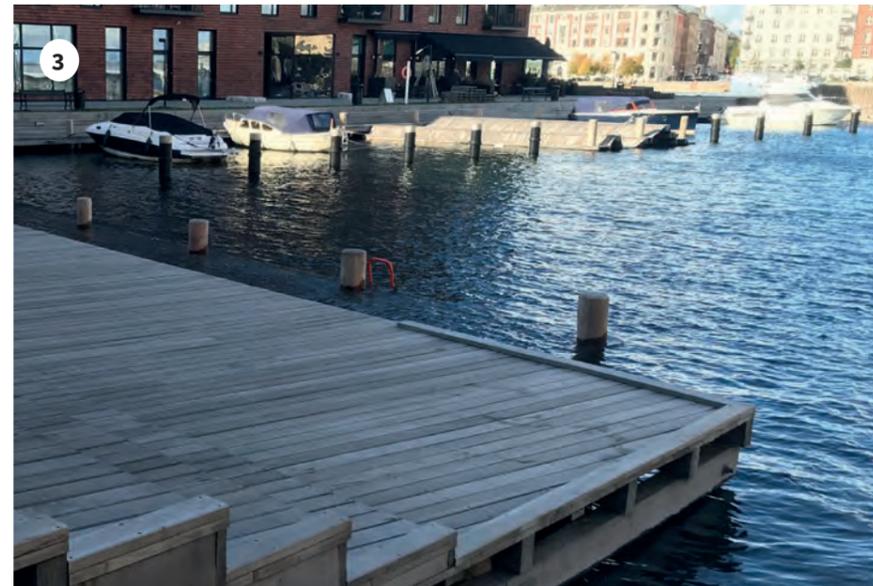
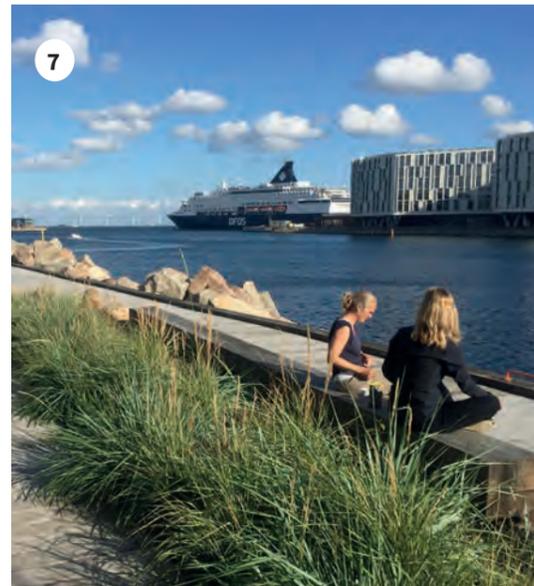
- Public transport corridor
- Potential Public transport stop
- Primary Cycle route
- Potential cycle storage hub



2.1 Masterplan

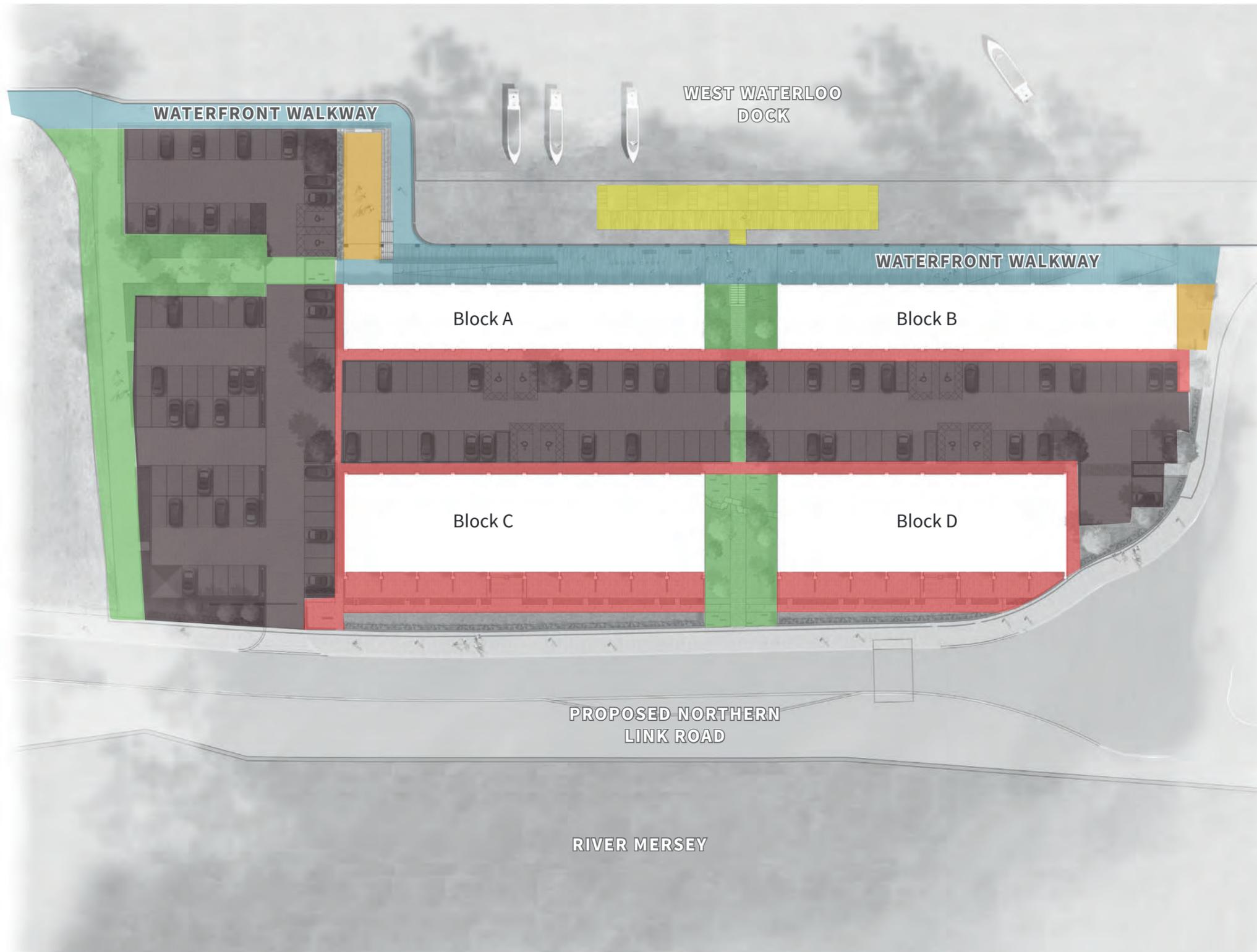


2.2 Masterplan Precedents





3.1 Spatial strategy



The development has been divided into a number of key areas:

- The **Waterfront Walkway** and cycle route. This route has a number of points to sit and relax along the waters edge. The timber jetty is accessed from a central point along the walk and could provide a docking point for a water taxi.
- The **car parks** across the development will be fringed with planting and semi-mature trees.
- **Green routes.** Key pedestrian and cycle routes have been wrapped in soft landscape to provide green corridors across the site. The central green route provides a cross connection between Northern Link Road and the waterside walk and the materials incorporate the original dock walls to showcase the heritage of the site.

KEY

- Green route
- Commercial / cafe area
- Car parking/ Carriageway areas
- Timber jetties
- Building perimeter/ access
- Waterfront walkway/ cycle route

3.2 Pedestrian movement



PEDESTRIAN MOVEMENT

The landscape proposals have been coordinated with the Liverpool Waters Masterplan and emerging Central Docks Neighbourhood Masterplan. A key design driver for the Masterplan is to provide a pedestrian and cycle connections from Princes Dock through to the Northern Docks.

The C02 site will provide a waterfront walkway and cycle route through the development along West Waterloo Dock. The proposed route is 6m in width, cantilevered over the waterway, wide enough to accommodate cyclists and pedestrians, whilst providing adequate space for pausing along the water edge. A floating timber jetty for canal boats and a water taxi is fixed to the waterfront walkway.

Pedestrians also have the option to walk along the River Mersey, along the proposed Northern Link Road (Jesse Hartley Way), a route with pedestrian and cycling provision.

There are two pedestrian connections between the waterfront walkway and Proposed Northern Link Road in the north and south of the development.

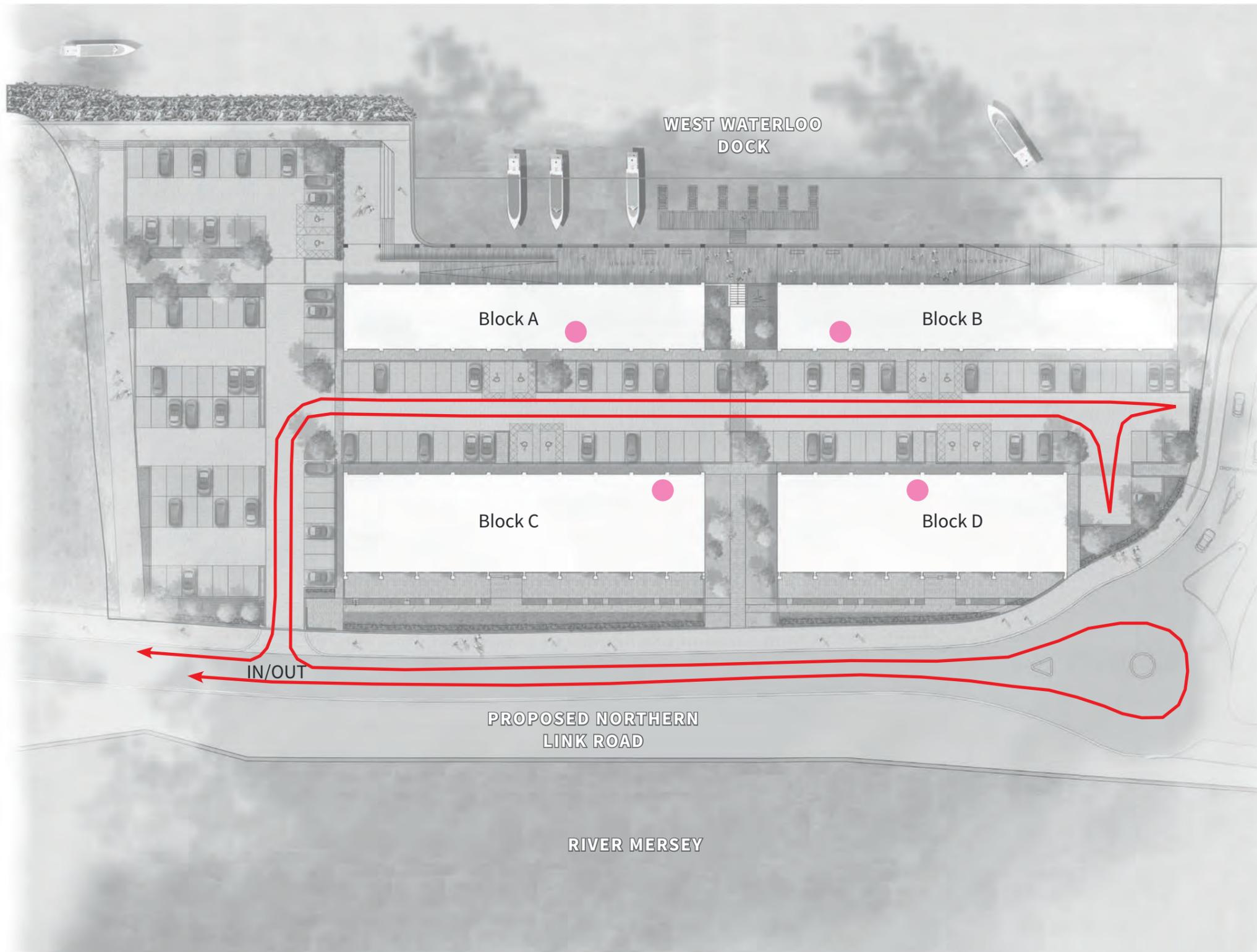
Each building block acts as a domestic connection spanning between the west and east of the site.

The bridge connection between the Isle of Man site and C02 will be fully coordinated with the Isle of Man design team.

KEY

- Waterside walk / cycle route - usable widths
- Proposed Northern Link Road / pedestrian / cycle route by others
- Main cross connections - unrestricted suitable for public use
- Domestic connections - unrestricted access designed for communal use
- Pedestrian crossing point
- ← Access at the gradient of 1:21

3.3 Servicing strategy



Following an emerging service strategy for Peel Group and the Liverpool Waters Masterplan, all refuse collections must be taken from within the development area.

- The refuse vehicle gains access into the site from the proposed northern link road. Each of the four building blocks has a ground floor bin store located on the internal carpark edge. The vehicle leaves the development from the same egress and uses the turning head on the eastern side of the development to turn around.

Refer to the transport consultants plan for further information.

- KEY**
- ➔ Refuse Vehicle Route
 - Bin store collection

3.4 Levels plan



The levels plan has a number of key constraints and opportunities to consider. The following strategy is applied:

- The building floor levels are set by the proposed Northern Link road levels. The building floor levels also respond to the Liverpool Waters Flood Risk Assessment.
- To enable access down to a suitable level which will gain interaction with the water, steps will connect a direct footpath from the Northern Link Road. Access is then gained onto the timber jetty via a ramp.
- All access is Part M compliant from the north and south of the development. There is step free access between the proposed northern link road and the waterside walk from the north and south. The timber jetty in the north has a ramp leading to the walkway from both eastern and western approaches.
- An open area for commercial spill out has been provided to the west of the waterside walkway. This will offer views towards the Liverpool waterfront and Royal Liver Building. Access to this is level from the Northern Link Road and ramps and stairs will lead to it from the timber walkway.
- The interface between the Isle of Man ferry site will be studied in detail with their design team to ensure a smooth transition.

KEY

- +4.77WL Water level
- FFL +8.500 Building floor level
- +6.50 Proposed spot height
- +8.10 Spot height (Isle of Man) To be reviewed post planning
- (+6.00) Existing level (retained)
- +6.00 TOW Top of wall / upstand and railing
- 1:60 Gradient / fall
- Steps
- Ramp

3.5 Car and cycle parking strategy

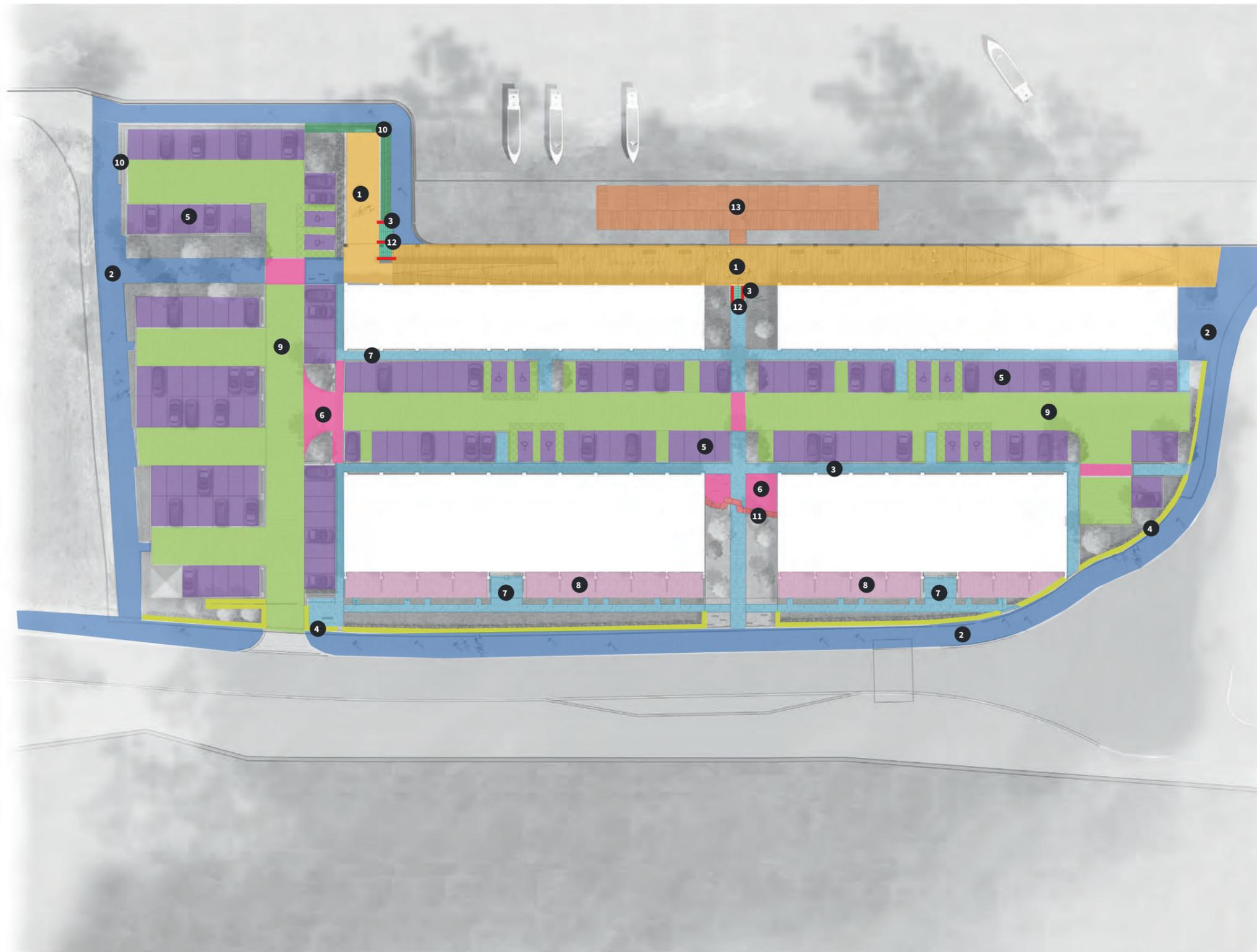


- Total car parking number within the site equates to 165 external and 14 internally within car garages.
 - There are 280 cycle parking bays internally distributed across the site with parking in each block.
- Refer to the Architect's site plan for further information.

KEY

- Car parking
(Total number = 165 spaces)
- Accessible parking bays: 10
- Electric charging bays: 20 (one of which accessible)
20 electric bays, 10 delivered now 10 future proofed for installation at a later date (inc disabled bays).
- Internal cycle parking provision
- Visitor cycle parking (3 hoops per location)





- All materials will be in keeping with the vision for Liverpool Waters and Central Docks Masterplan. The waterside walkway is proposed as composite timber decking. All materials are proposed to be robust and low maintenance .
- The surfacing to parking areas to the north and south are proposed as exposed stone aggregate paving.

KEY

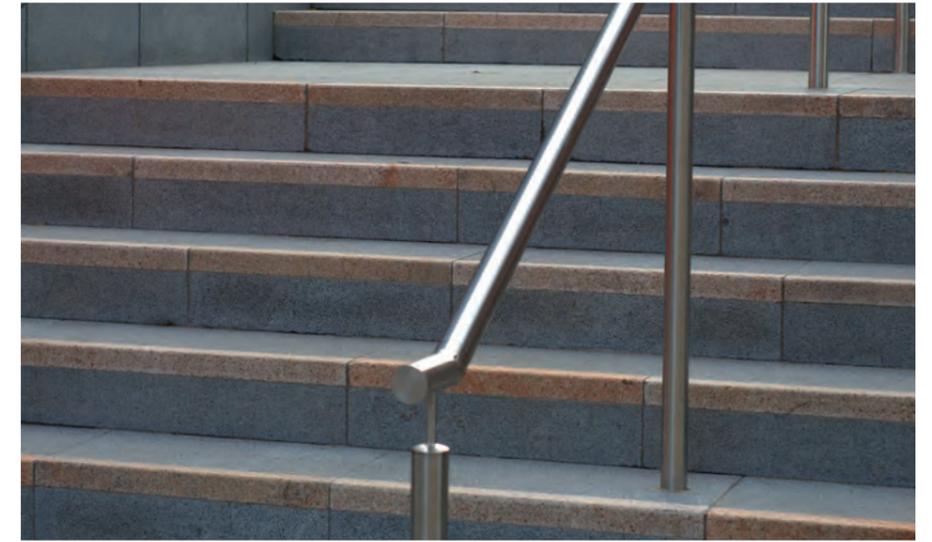
- 1 Timber walkway decking
- 2 Granite slab paving to match footpaths on Northern Link Road public walkway along IOM site
- 3 Granite steps
- 4 Brick walls with metal elements
- 5 Precast exposed stone aggregate flag paving
- 6 Precast exposed stone aggregate block paving
- 7 Precast exposed stone aggregate block paving
- 8 Precast exposed stone aggregate block paving
- 9 Precast exposed stone aggregate flag paving
- 10 Gabions
- 11 Retained Granite dock quay wall
- 12 Handrails
- 13 Timber jetty



TIMBER JETTY/TIMBER PLATFORMS



GRANITE SLAB PAVNG TO TIE IN WITH IOM SITE



GRANITE STEPS



BRICK WALL WITH METAL ELEMENTS



PRECAST EXPOSED STONE AGGREGATE FLAG PAVING

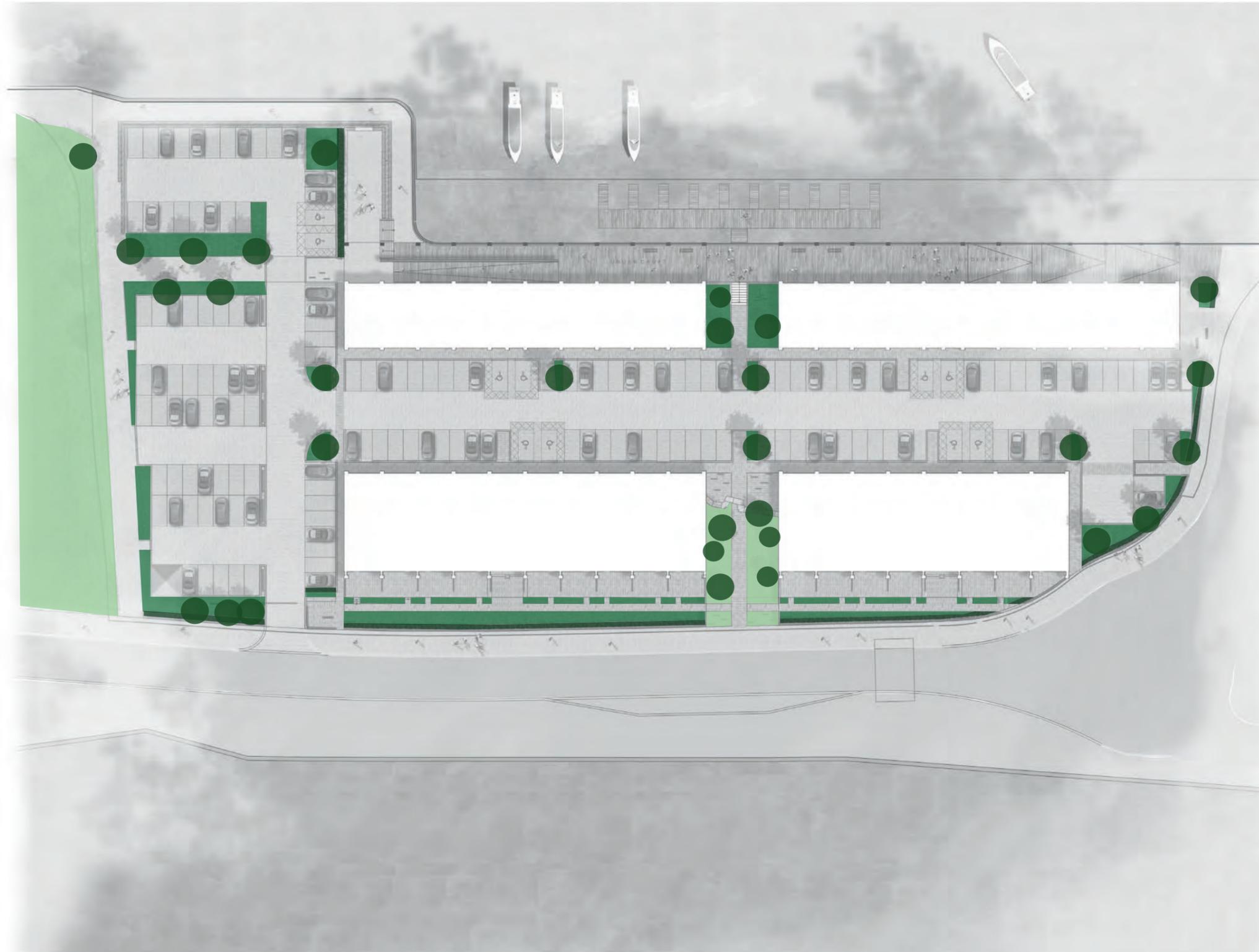


PRECAST EXPOSED STONE AGGREGATE BLOCK PAVING



SEATING WALLS

4.1 Planting strategy



- The planting plan aims to establish a robust mix of tree planting that will withstand the coastal conditions. The site is highly exposed to the prevailing winds from the south-east and north-west.
- Trees are proposed to be planted within groups to provide protection against the wind.
- All planting is proposed to be low maintenance and robust.
- Trees to maintain views through the site.
- Grasses and herbaceous planting to provide seasonal interest.
- The planting schedule will aim to replicate existing short perennial coastal vegetation through the use of loose substrates and hardy, salt tolerant species. The substrates and species selected will provide habitat opportunities for a range of invertebrates, including declining pollinating species. This in turn will provide a feeding resource for birds and small mammals, including the nationally scarce black redstart, a bird species of industrial and coastal habitats that is listed on Schedule 1 of the Wildlife and Countryside Act 1981 (as amended)
- A list of species have been provided by the Ecology Consultant which will be explored in further detail post planning for inclusion:

KEY

- Proposed trees
- Grass
- Herbaceous planting
- Hedging

TREES



SALIX ALBA 'LIEMPDE'



PINUS MARITIMA

PERRENIALS AND GRASSES



STACHYS BYZANTIA 'BIG EARS'



FESTUCA GLAUCA



AJUGA REPTANS



HEBE BRACHYSIPHON



LUZULA NIVEA



LEYMUS ARENARIUS

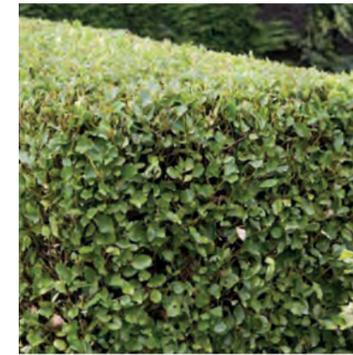


PANICUM VIRGATUM



VERBENA BONARIENSIS

HEDGES

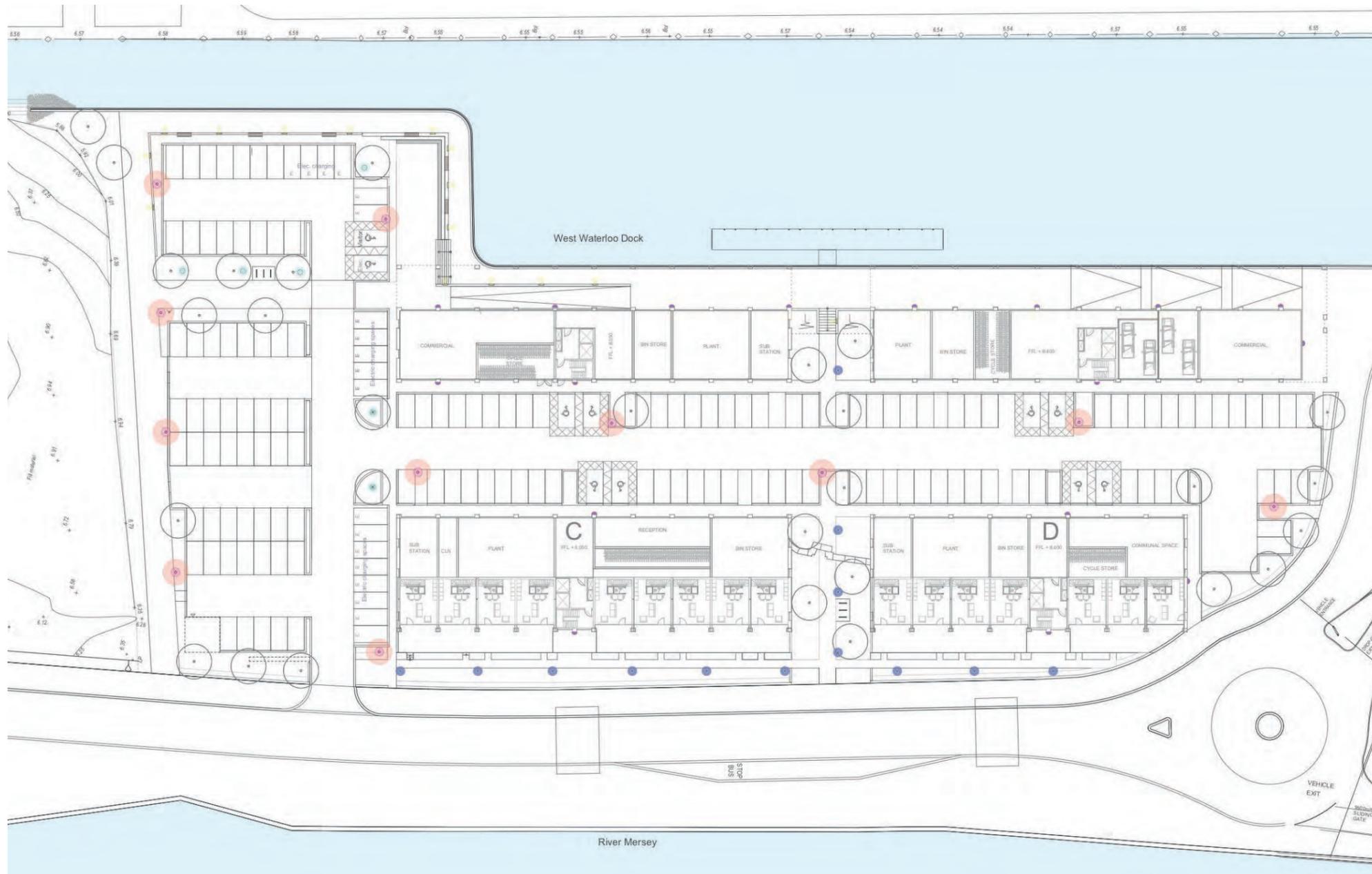


GRISELINIA LITTORALIS



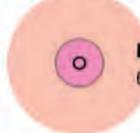
SEA PINK THRIFT

4.2 Lighting strategy



LIGHTING

Strategic design only, subject to qualified engineer's review and specification/detail design

- 
Lighting Columns
6m high
- 
Lighting bollard
850mm high
- 
Building mounted bulk-head downlight
- 
Wall mounted (recessed) light
to shed light onto paths and steps
- 
Tree uplight
buried flush with adjacent surface

4.3 Street furniture strategy



The street furniture proposals follow these principles:

- Reuse reclaimed materials when possible.
- Furniture will be chosen to be robust and low maintenance.
- To be in keeping with the hard materials strategy to be industrial and contemporary in character.
- Seating is proposed at key points within the site for public and visitor use.
- Railings provide protection from falling into the water.

KEY

- Seats
- Walls
- Gabions
- Handrails
- Balustrade
- Wayfinding fingerposts (subject to detailed design)
- Cycle stands

4.4 Wind mitigation strategy



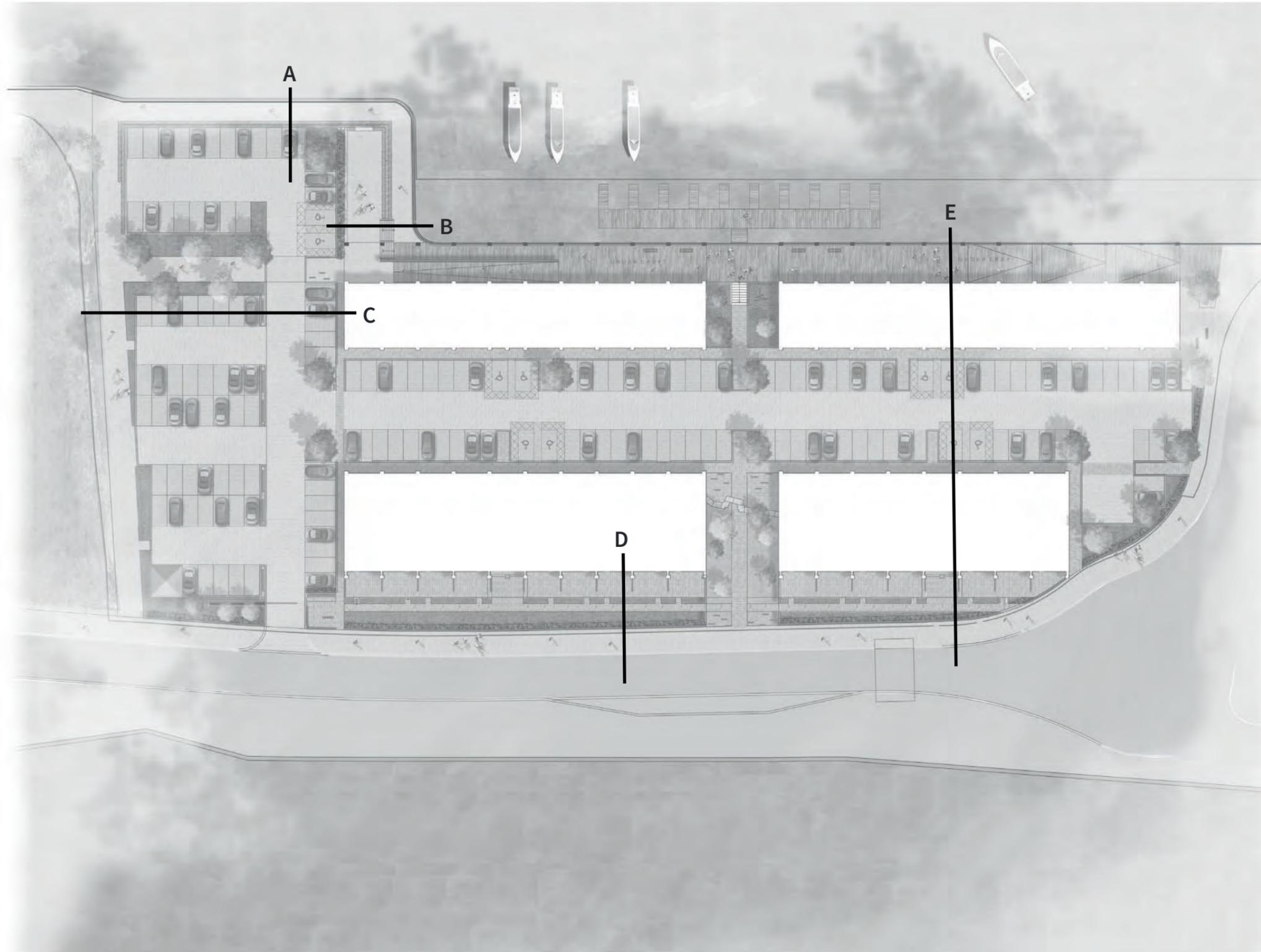
- The wind mitigation strategy is shown as indicative and is subject to change inline with architect's and wind specialist input.

KEY

- TYPE A
2.5m (w) x 2.5m (h). 50% porous.
- TYPE B
1m (w) x 2.5m (h). 50% porous.
- TYPE C
1.5m (l) x 2.5m (w). 50% porous.
- TYPE D
1.5m (w) x 1.8m (h). 50% porous.
- TYPE E
3m (w) x 3m (h). 50% porous.



5.1 Section location plan

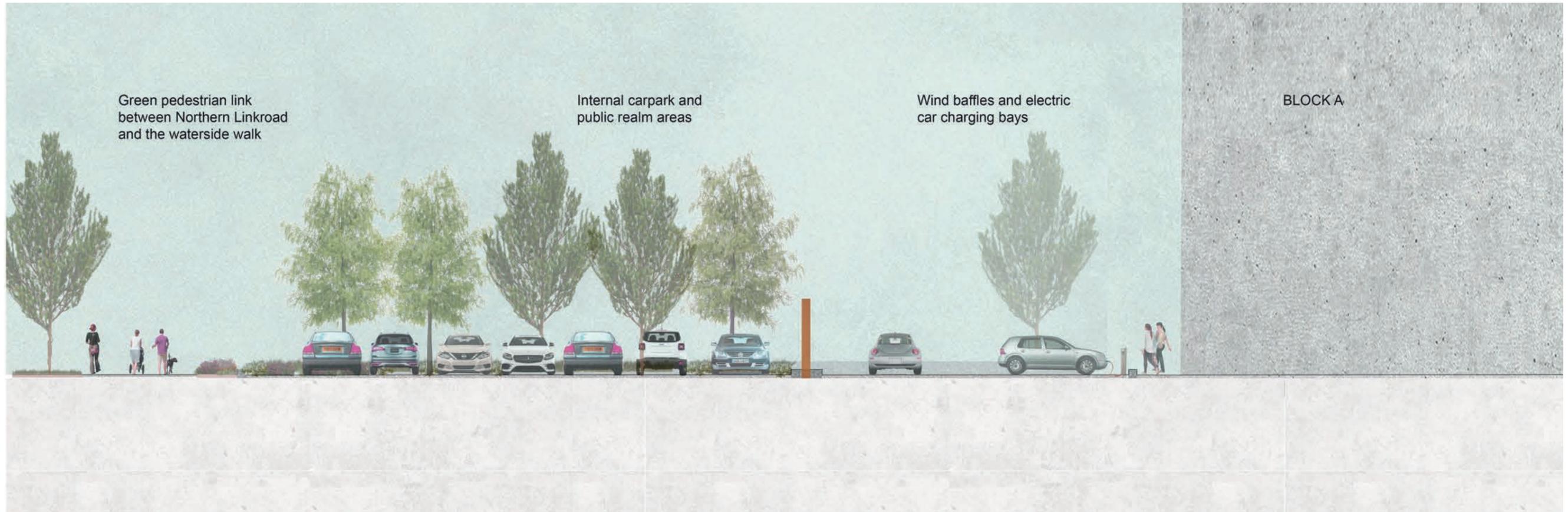




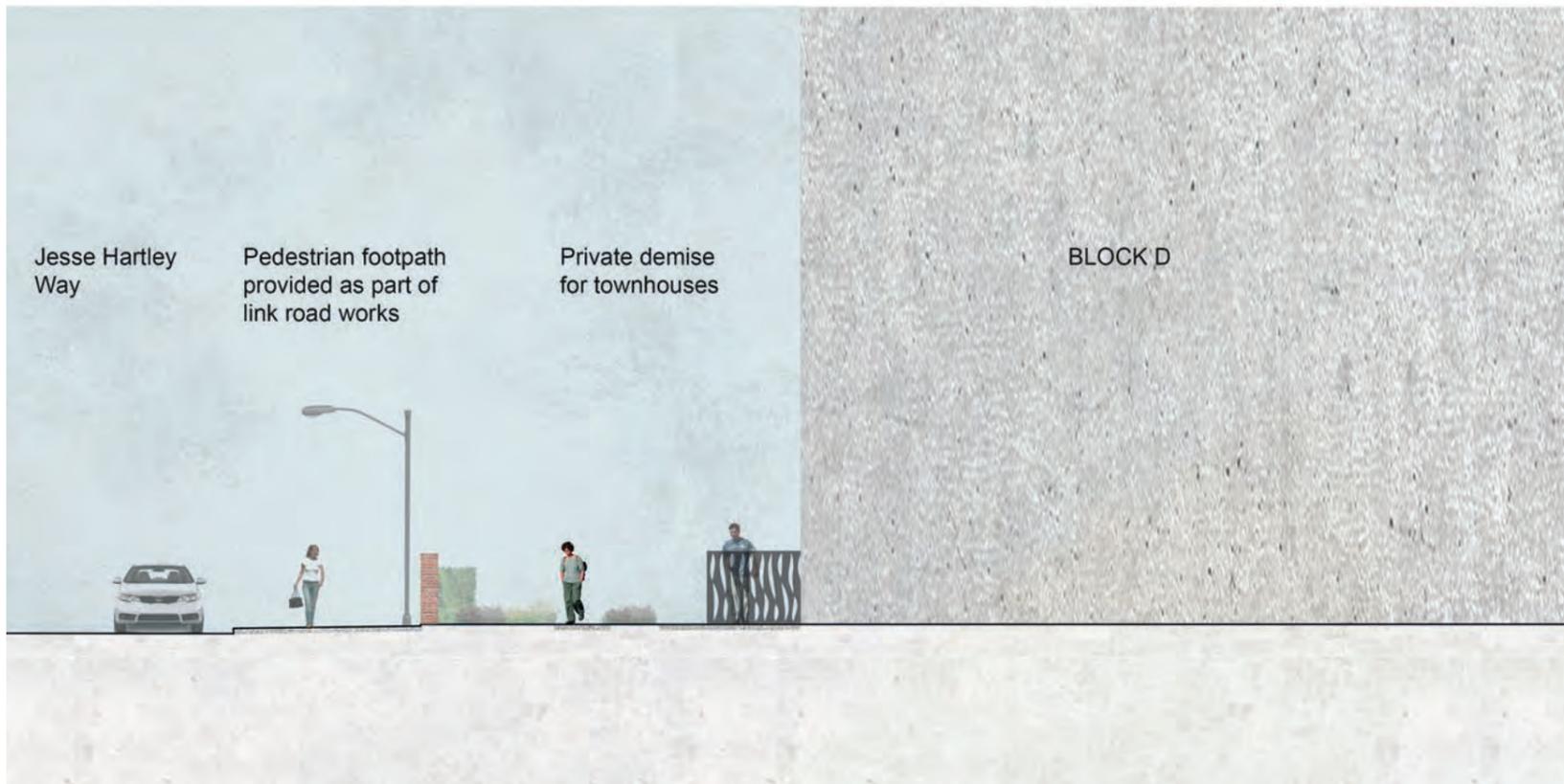
LANDSCAPE SECTION A, NOT TO SCALE



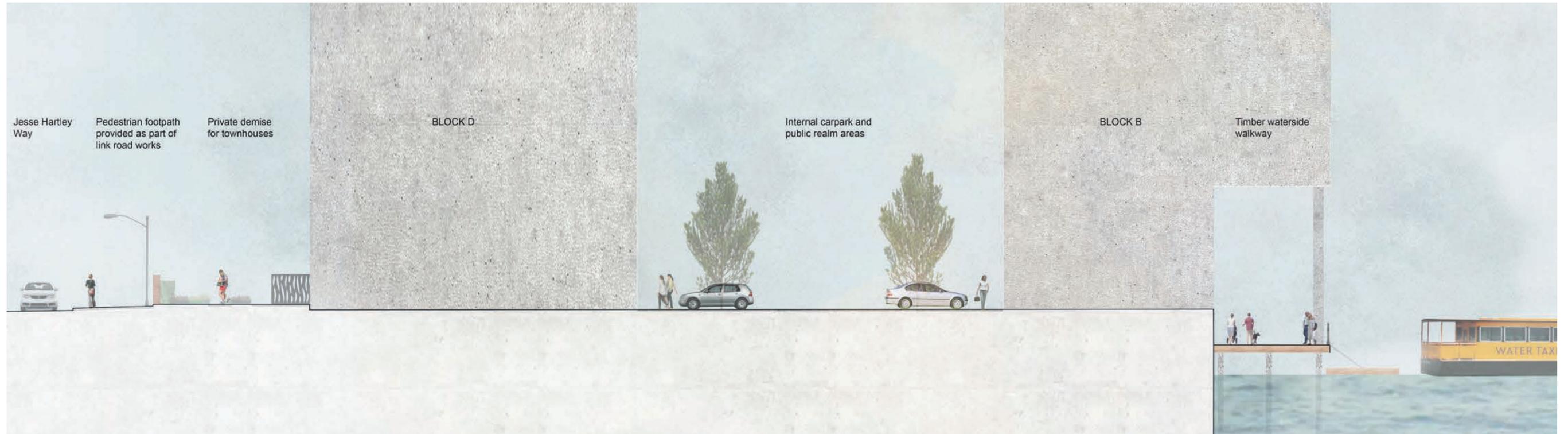
LANDSCAPE SECTION B, NOT TO SCALE



LANDSCAPE SECTION C, NOT TO SCALE



LANDSCAPE SECTION D, NOT TO SCALE



LANDSCAPE SECTION E, NOT TO SCALE



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